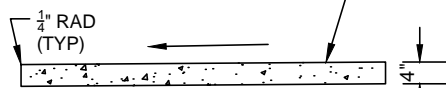
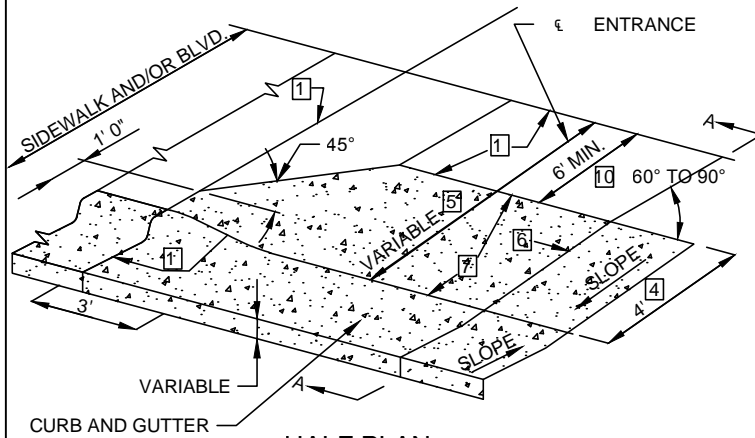


0.015 FT./FT. ($\frac{3}{16}$ " PER FT.) TYPICAL TOWARD ROADWAY

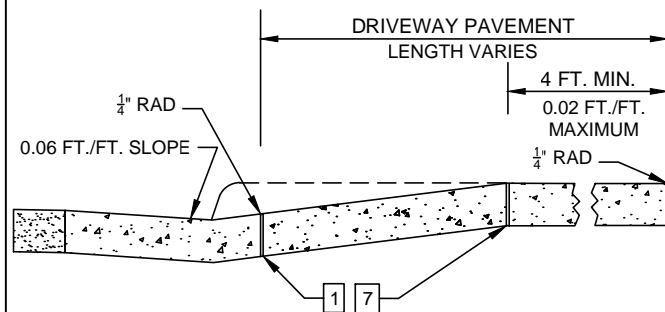


SECTION THRU WALK

CONCRETE WALK



HALF PLAN PERSPECTIVE



SECTION AA CURB RETURN

NOTES:

- SEE ROAD DESIGN MANUAL, CHAPTER 5, FOR GEOMETRIC DESIGN OF ENTRANCES.
- WHERE THE MAX. ALLOWABLE ENTRANCE GRADIENT WOULD BE EXCEEDED, DUE TO THE POSITION OF EXISTING WALK, THE WALK SHALL BE REMOVED AND REPLACED, OR THE PAVEMENT WARPED TO PROVIDE THE REQUIRED ENTRANCE SLOPE.
- SEE PLANS FOR PLACEMENT OF WALK AND DIMENSIONS FOR CONSTRUCTION OF DRIVEWAYS.
- NO DEDUCTION SHALL BE MADE IN CURB AND GUTTER FOR ENTRANCE.
- 1. $\frac{1}{2}$ " EXPANSION JOINT. $\frac{1}{2}$ " PREFORMED JOINT FILLER MATERIAL, AASHTO M 213 (REQUIRED WHEN 2" CONCRETE AREAS ARE POURED SEPARATELY).
- 2. $\frac{1}{2}$ " EXPANSION JOINTS AT 60' (APPROX.) MAXIMUM INTERVALS.
- 3. MATCH IN PLACE DRIVEWAY THICKNESS (6" MIN.).
- 4. WITHOUT SIDEWALK, PAVE ONLY TO THE ENDS OF CURB RETURN WHEN ENTRANCE IS UNSURFACED OR CONSTRUCTION IS NOT NEEDED BEYOND THIS POINT.
- 5. WITH SIDEWALK, PAVE TO THE BACK OF THE SIDEWALK. PAID FOR AS CONCRETE DRIVEWAY PAVEMENT.
- 6. CONTRACTION JOINT (FORMED OR SAWED).
- 7. EXPANSION JOINT NOT REQUIRED IF ADJACENT SECTIONS ARE POURED MONOLITHICALLY. SEE SECTION A-A.
- 8. SEE PLANS FOR PLACEMENT OF PED. CURB RAMP.
- 9. FORM CONTRACTION JOINT AS NEEDED TO PRODUCE APPROXIMATELY SQUARE PANELS (MAXIMUM WIDTH 15' BETWEEN JOINTS).
- 10. THE MINIMUM CONTINUOUS AND UNOBSTRUCTED CLEAR WIDTH OF A PEDESTRIAN ACCESS ROUTE SHALL BE 4'.
- 11. SEE PLANS FOR PROPOSED CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE, WHICH MAY NOT EXCEED 0.02'/FT. AS CONSTRUCTED.

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ENGINEERING DEPARTMENT

DRIVEWAYS, SIDEWALKS AND TRAILS

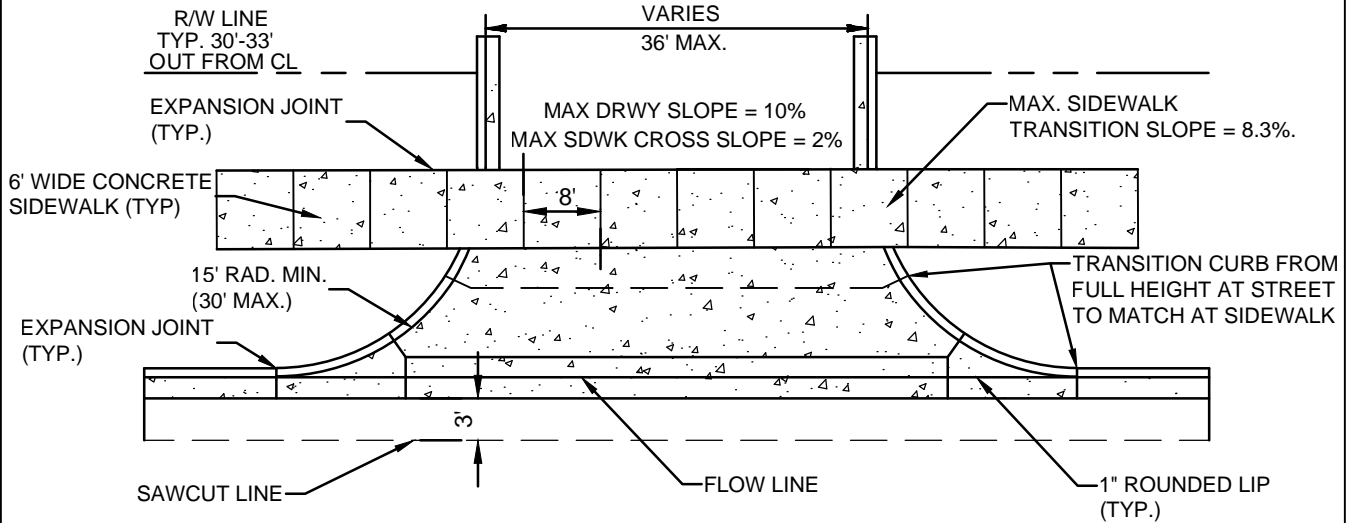
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P-1

COMMERCIAL DRIVEWAY WITH BOULEVARD SIDEWALK

NOTES:

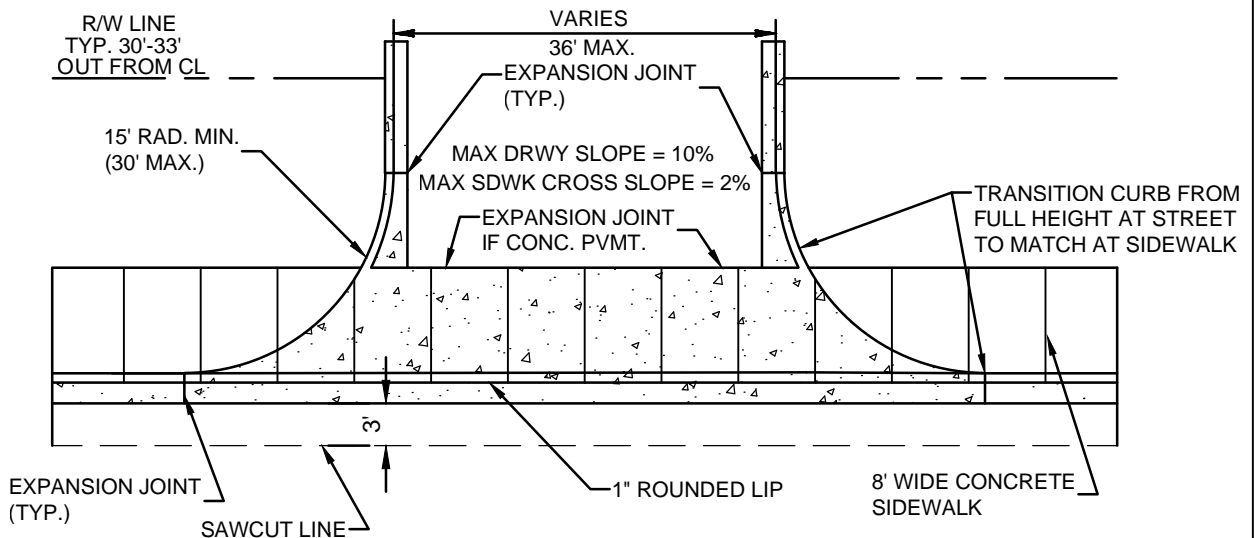
1. CONSTRUCT DRIVEWAY WITH 8" CONCRETE AND 6" AGGREGATE BASE.
2. IF BITUMINOUS, CONSTRUCT DRIVEWAY IN 2-2" LIFTS.



COMMERCIAL DRIVEWAY WITH SIDEWALK

NOTES:

1. CONSTRUCT DRIVEWAY WITH 8" CONCRETE AND 6" CLASS 5 AGGREGATE BASE.
2. IF BITUMINOUS, CONSTRUCT DRIVEWAY IN 2-2" LIFTS.



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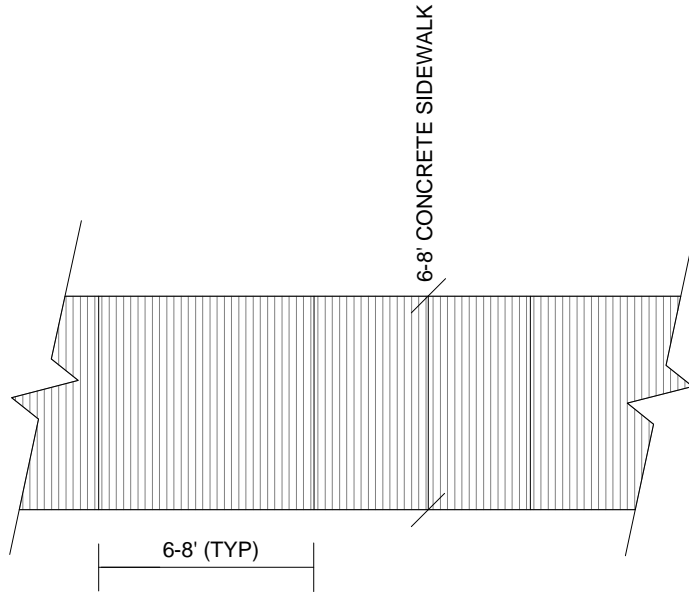


ENGINEERING DEPARTMENT

COMMERCIAL DRIVEWAY

PLATE NUMBER

P-2



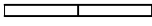
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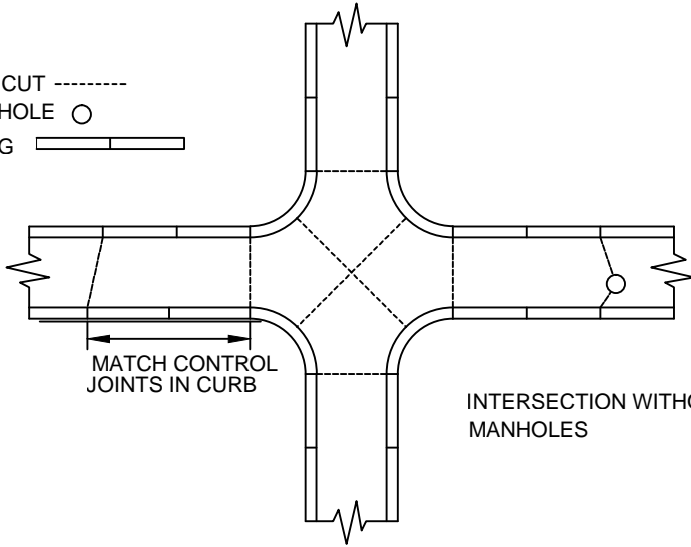
- NO GREATER THAN 2% CROSS SLOPE IN DIRECTION OF POSITIVE DRAINAGE FLOW ON ALL WALKS UNLESS OTHERWISE NOTED.
- BROOM FINISH PANELS (DIRECTION OF FINISH SHALL BE DETERMINED IN FIELD BY ENGINEER PRIOR TO POUR).
- CRACK CONTROL JOINT (REPLACE WITH EXPANSION APPROXIMATELY EVERY 30 LF OF WALK AND/OR AS DETERMINED IN FIELD BY ENGINEER).

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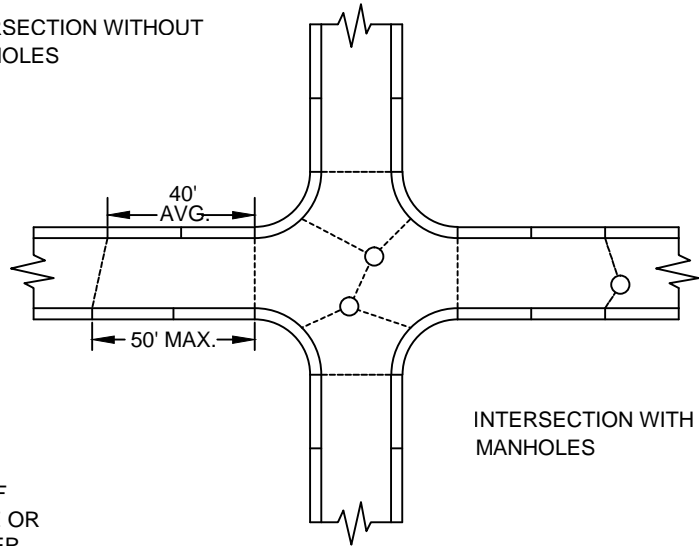

ENGINEERING DEPARTMENT
CONCRETE SIDEWALK
JOINT PATTERN

PLATE NUMBER
P-3

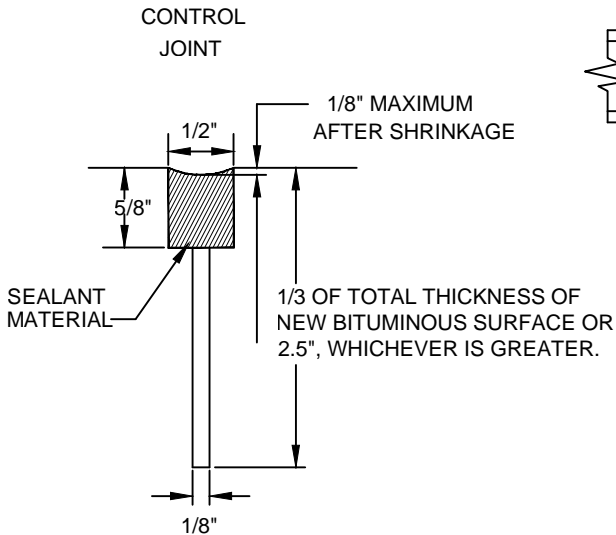
SAW CUT -----
 MANHOLE ○
 C & G 



INTERSECTION WITHOUT MANHOLES



INTERSECTION WITH MANHOLES



NOTES:

- A. THE CUT LOCATIONS SHALL BE AT MANHOLES AND SPACED BETWEEN 40 AND 50 FEET APART OR AS DIRECTED BY PROJECT ENGINEER.
- B. THE CUT SHALL FOLLOW A STRAIGHT LINE OBTAINED BY THE USE OF A SNAP LINE.
- C. TRANSVERSE CRACK CONTROL JOINTS SHALL START AND STOP AT CONTROL JOINTS IN CURB & GUTTER.
- D. THE CUT SHALL BE A WET CUT BY A PAVEMENT SAW AT A WIDTH OF 1/8" AND A DEPTH 1/3 OF PAVEMENT THICKNESS OR 2.5", WHICHEVER IS GREATER.
- E. DIRECTLY FOLLOWING THE CUTTING, THE CUT SHALL BE WASHED WITH A PRESSURE HOSE UNTIL THE WATER IS CLEAR, FROM THE CUT.
- F. THE CUT SHALL THEN BE DRIED WITH COMPRESSED AIR.
- G. THE FINAL STEP SHALL BE TO FILL WITH CRACK FILLER MEETING MN/DOT SPECIFICATION 3723.

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ENGINEERING DEPARTMENT

TRANSVERSE CRACK CONTROL JOINTS

PLATE NUMBER

P-4

CITY OF ROSEVILLE
 ENGINEERING DEPARTMENT
 (YEAR) PAVEMENT MANAGEMENT PROJECT

TRAVEL WITH CAUTION

CONTRACTOR
 ADDRESS
 TEL. NO.

City of Roseville
 2660 Civic Center Drive
 651-792-7003



TO BE FILLED OUT WITH
 LOW BIDDER INFORMATION

3' X 5'

1 SIGN AT INTERSECTION OF (ROAD NAME) AND (ROAD NAME)

DESIGN:	DATE: 3-08-07
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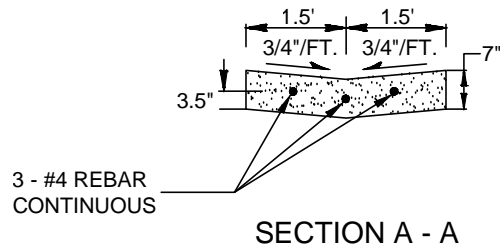
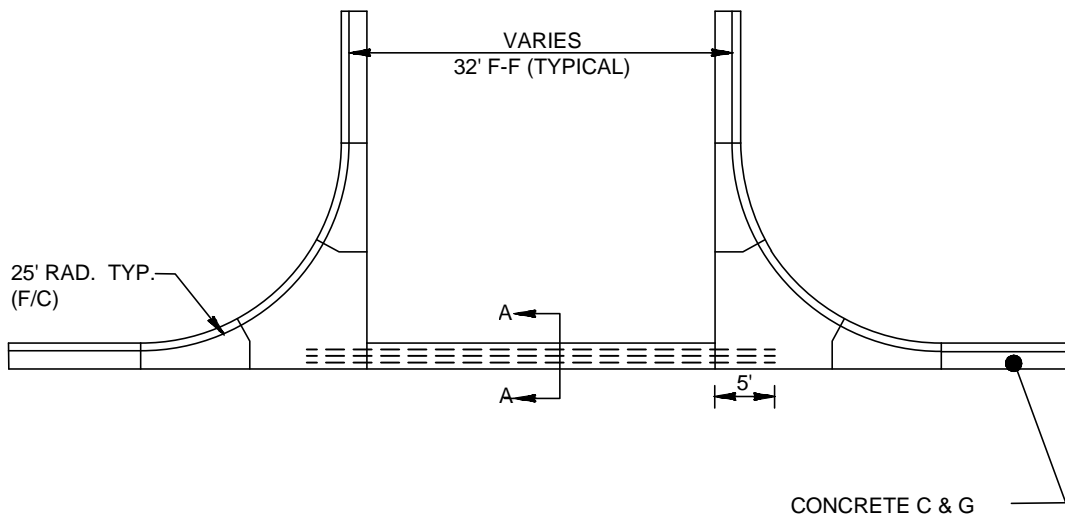


ENGINEERING DEPARTMENT

CONSTRUCTION SIGN

PLATE
 NUMBER

P-5



NOTES:

1. NO CONTRACTION JOINTS IN VALLEY GUTTER.
2. THE CONCRETE VALLEY GUTTER SHALL BE CONSTRUCTED ON 8" AGGREGATE BASE, CLASS 5.

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ENGINEERING DEPARTMENT

CONCRETE VALLEY GUTTER

PLATE NUMBER

P-6